

Scottish Borders Council  
Supplementary Planning Guidance  
Burgh Yard, Galashiels  
September 2011





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# 1 INTRODUCTION

## Purpose of Brief

- 1.1 This brief sets out guidance in relation to the redevelopment of the former Burgh Yard site in Galashiels town centre. It seeks to:
  - identify the main constraints on the site
  - establish spatial principles for development of the site
  - provide an outline framework for future redevelopment, in terms of urban design principles, land use options and planning issues.
  - identify anticipated requirements for development contributions
- 1.2 The subheadings below indicate where detailed attention is required to address specific issues and where development contributions will be sought. This brief should be considered in conjunction with appropriate planning guidance given at the local and national level. A selection of related policy advice and guidance is referenced within this document, but this should not be regarded as exhaustive.
- 1.3 This development brief has been prepared in order to explicitly set out the position of the local planning authority. It is intended to enhance understanding of policy requirements and streamline the planning and development process.

## Objectives

- 1.4 Burgh Yard is located on one of the key approaches to Galashiels (see Fig 1: Location Plan). It's strategic prominence necessitates a high quality design response incorporating sustainable development principles that achieve the following objectives:
  - an appropriate form of buildings and spaces
  - an appropriate quality of design commensurate with its strategic town centre location
- 1.5 The brief sets out a Development Vision comprising a physical framework and a range of appropriate potential land uses for the site. The main objectives are to promote sensitive development as a high quality addition to the town centre, provide clarity on the development principles required by Scottish Borders Council and allow for flexibility and innovation in the future redevelopment of the site.

## 2 SITE CONTEXT AND DESCRIPTION

### Site description

2.1 The site is situated at the former Burgh Yard site in Galashiels town centre, comprising 1ha of brownfield land. It is bounded to the north by Huddersfield Street and Paton Street and to the east by Hill Street. The construction of Braw Lads Brae, a new section of principal road forming part of the Galashiels Inner Relief Road (GIRR 3), has created 3 distinct areas (as numbered on Figure 2), each comprising:

1	Braw Lads' Brae/Hill St site:	0.84ha
2	Paton Street site:	0.092ha
3	Albert Place site:	0.084ha
	<b>Total area:</b>	<b>1.016ha</b>

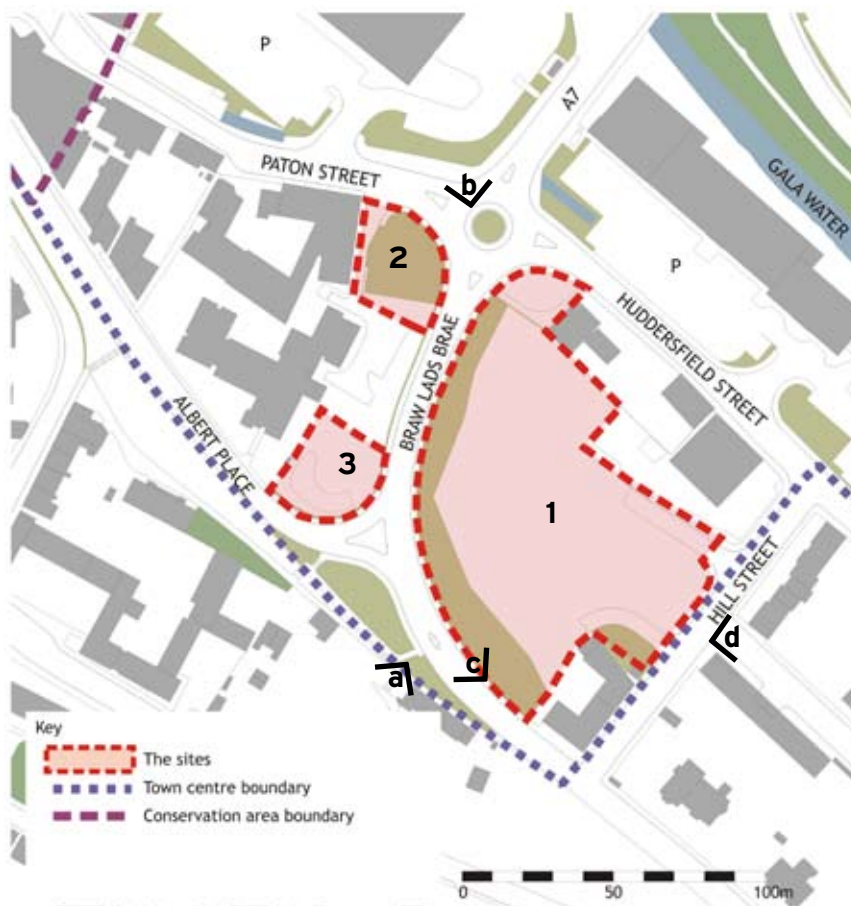


Figure 1 - Site location in Galashiels Town Centre



view d



view c



view a



view b



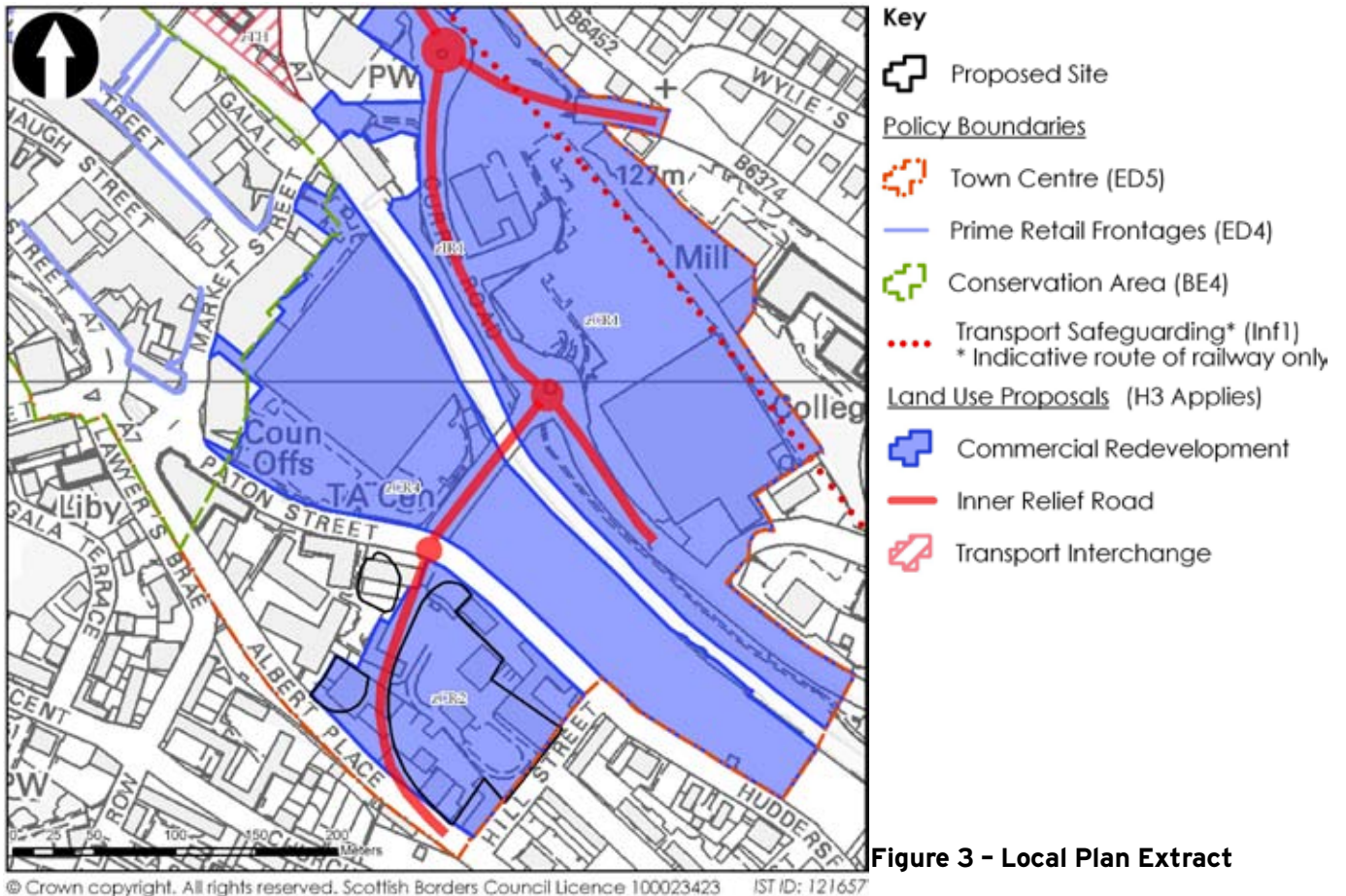
2.2 The site has an approximate 13.0m change in level, from a high point at the southern corner to the lower end at Huddersfield Street. However, the southern corner site, 77 Albert Place, and properties along the south side of Huddersfield Street are not included within the subject site area.

**Context: land use + movement**

2.3 The site is situated between residential areas to the south and east, the adjacent sheltered housing and the town centre/Gala Water retail park to the west and north respectively. It forms a transitional point between the town centre and surrounding neighbourhoods. Pedestrian movement between the two is therefore a key consideration. The vehicular network serving the site is dominated by Braw Lads Brae, part of the Galashiels principal road network. The site is situated within 650m walking distance of the proposed Transport Interchange & proposed Borders Railway line, which will connect Galashiels to Edinburgh and the Lothians.



Figure 2 - Site context: land use + movement



**Figure 3 - Local Plan Extract**

### Local Plan Policy

2.4 The majority of the Burgh Yard site is covered by a commercial redevelopment policy zoning (zCR2) within the Scottish Borders Consolidated Local Plan (2011), and amounts to 1.02 hectares (see Fig 3 Local Plan Extract). Following completion of this section of the Galashiels Inner Relief Road (GIRR) that bisects the site, an additional area of land to the west of the road is now available for redevelopment which could appropriately be developed in conjunction with the rest of the Burgh Yard site and as such is included in this brief. The site lies within the central area of Galashiels where policies ED5 and H3 apply. Policy ED5 provides for retailing and other appropriate town centre development provided that the character, vitality, viability, and mixed-use nature of the town centre is maintained and enhanced. Appropriate town centre development, other than shopping, includes food and drink uses, offices, commercial, leisure and entertainment, residential, health care, education, and tourism related uses. The policy states that if any proposed development would create an unacceptable adverse impact on the town centre then it would be refused. Policy H3 allows for commercial redevelopment within classes 1- 4 (of the Use Classes Order). Section 5 of the policy sets out the criteria by which exceptions can be considered, including the provision of significant community benefit outweighing the need to maintain the original proposed use.

### Previous Planning History

2.5 When the site was previously being considered for disposal by the Council in 2000 planning consent was given for most of the site for non-food retail with a drive-through restaurant. This was prior to the Waverley Mill development being considered and completion of the adjacent section of the Galashiels Inner Relief Road (GIRR).

### Site Ownership

2.6 The site is owned by Scottish Borders Council. The access lane adjacent to the Paton Street area is privately owned, as is the area of land between the site and Huddersfield Street.



## Constraints

2.7 Major constraints on the use of the site, as outlined below and on Figure 4, are:

- Topography
- Access / parking
- Contamination
- Views

<p><b>Topography</b></p>	<p>The site is constrained by the steep topography both within the site boundary and on adjacent land:</p> <ul style="list-style-type: none"> <li>• Braw Lads Brae/Hill St - there is a steep embankment along Braw Lads Brae falling towards Hill Street. Hill Street is also on a steep gradient.</li> <li>• Paton Street - the site is relatively unconstrained by landform.</li> <li>• Albert Place site - constrained by steep concave landform that takes in the whole site.</li> </ul>
<p><b>Access</b></p>	<ul style="list-style-type: none"> <li>• Braw Lads' Brae/Hill St - access is currently only from Hill Street. Opportunities for additional access are restricted due to site ownership, road geometry and topography.</li> <li>• Paton Street - access can be taken of Paton Street but this is via the private access serving Abbotsford Court, use of which would be subject to negotiation with Cairn Housing. Opportunities for parking are limited - there is potential for some on-site parking on the western edge of the site, subject to access from the adjoining private road.</li> <li>• Albert Place site - access is severely limited by the topography and adjacent ownership. Access could potentially be provided via the private road serving Abbotsford Court, use of which would be subject to negotiation with Cairn Housing. Opportunities for parking are very limited.</li> </ul>
<p><b>Contamination</b></p>	<p>The site contains contamination from historic industrial land uses such as the former Mill and, more recently, the former petrol station. The nature and extents of this was assessed as part of the GIRR 3 development, however further survey will be required.</p>
<p><b>Views</b></p>	<p>The site is visually prominent when viewed from the neighbourhoods situated on the steep valley sides to the north and south. The site also forms a key focal point marking arrival into central Galashiels when travelling northwards along the A7 and adjacent streets. See constraints plan below.</p>



Brownfield site: possible contaminants



Hill St access to site



steep topography at Braw Lads Brae



steep topography at Albert Place site



Paton Street site: constrained access



Albert Place site: constrained access

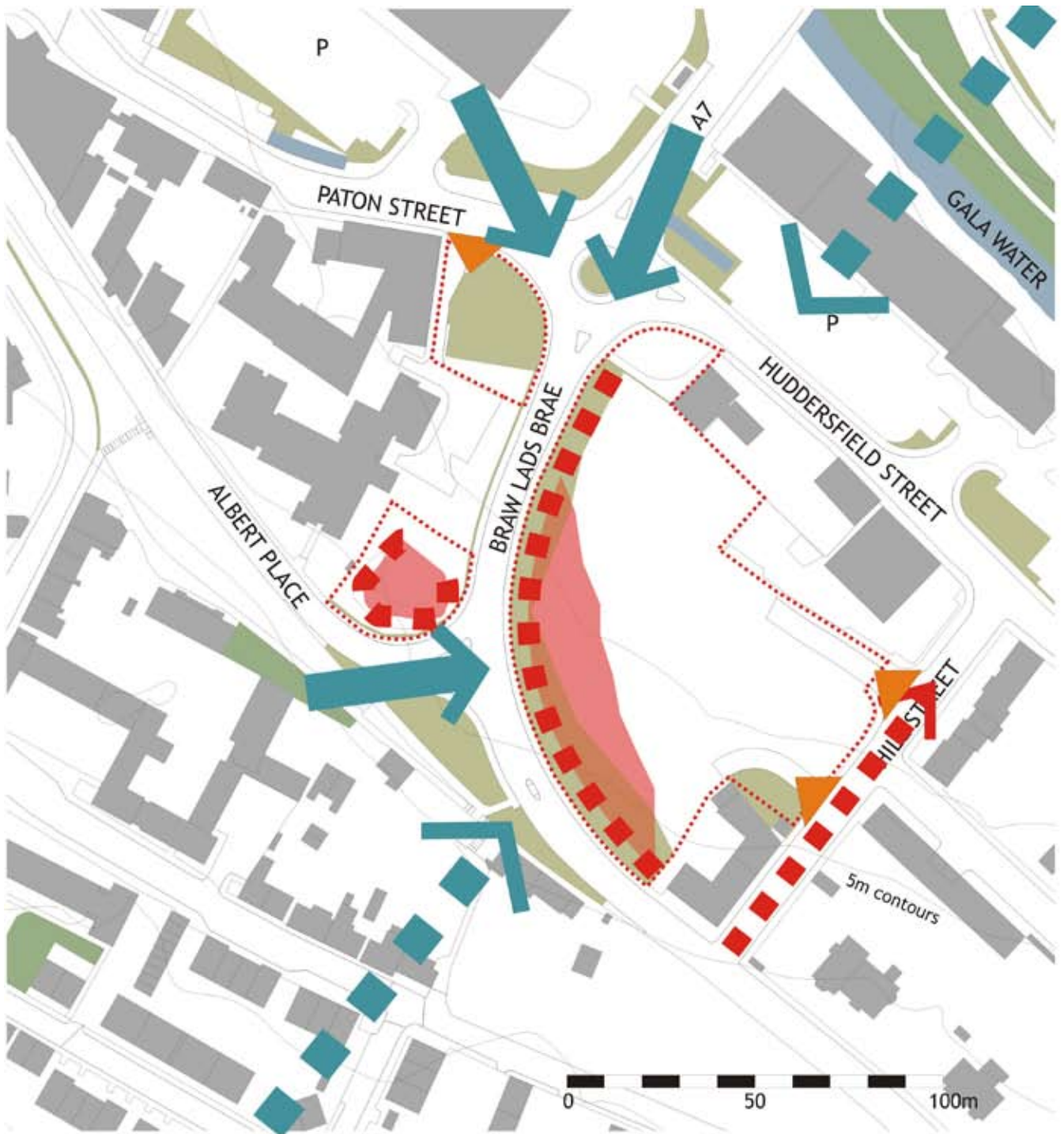


Paton Street site: constrained access



views from wider town





**VIEWS**



- From the neighbourhoods situated on the steep valley sides to the north and south
- From A7
- From adjacent streets

**TOPOGRAPHY**



- Steep embankment along Braw Lads Brae
- Steep gradient on Hill Street
- Albert Place - steep concave landform

**ACCESS**



- Restricted access to Paton Street site
- Severely restricted access to Albert Place site
- Limited access to Braw Lads Brae

Figure 4 - Constraints

### 3 URBAN DESIGN FRAMEWORK

Given the strategic importance of this town centre location and the Council's commitment to a sustainable high quality built environment, and reflecting the constraints and policy objectives previously outlined, cognisance of the following **Development Vision, Spatial Principles** and **Land use Options** is required.

#### **Development Vision**

- 3.1 An appropriate form of development would incorporate a signature frontage along the curved edge of the new road. There would also be a straight block addressing the Hill Street frontage. This form of perimeter block development is promoted through the council's Supplementary Planning Guidance on Placemaking & Design and in the Scottish Government's Policy Statement, 'Designing Streets' to encourage development with a strong civic presence that serves to enclose the street. The balancing of car parking requirements and quality public/private space within the site is critical. It is visually important to achieve a high standard of central amenity space.

The Paton St and Albert Place sites must provide a positive frontage to the street. The Paton Street site should present a sense of enclosure to the street by tying in with the adjacent building line and incorporating a strong built elevation to the roundabout. This will offset the relative openness of the adjacent retail park. The Albert Place site could be developed as an introverted residential development that is sympathetic to the landform and adjacent sheltered housing, or as a strong built elevation that positively addresses the street similar to that recommended at Paton Street.

Any planning application for this site must be accompanied by a Design Statement (PAN 69, SPG Placemaking & Design). This must set out a clear design rationale and demonstrate how the proposal has responded to the spatial principles set out in this brief. Proposals will be assessed against these principles.

#### **Spatial principles**

- 3.2 The key components of achieving this vision are outlined in summary below and on Figure 3 - Spatial Principles.

##### **1 Building Line**

- Strong urban form to create built frontage to Braw Lads Brae.
- Building line must tie in with the existing building line to the south (Porters' Porches/Tweed Design), creating continuity and a coherent streetscene.
- Development of the site should achieve overall continuity in the design of the elevation to Braw Lads Brae to ensure a strong civic presence and quality addition to the townscape of central Galashiels. Even if the site is subject to subdivision, development must consider this frontage in its entirety.
- Building design should incorporate key corner frontage onto Paton Street/Huddersfield Street to the west and east of Braw Lads Brae respectively.
- The design should safeguard the potential to extend the corner frontage at Huddersfield Street northwards in the longer term.
- Secondary building/site frontage must relate positively to the eastern boundary, Hill Street, continuing the existing building line.



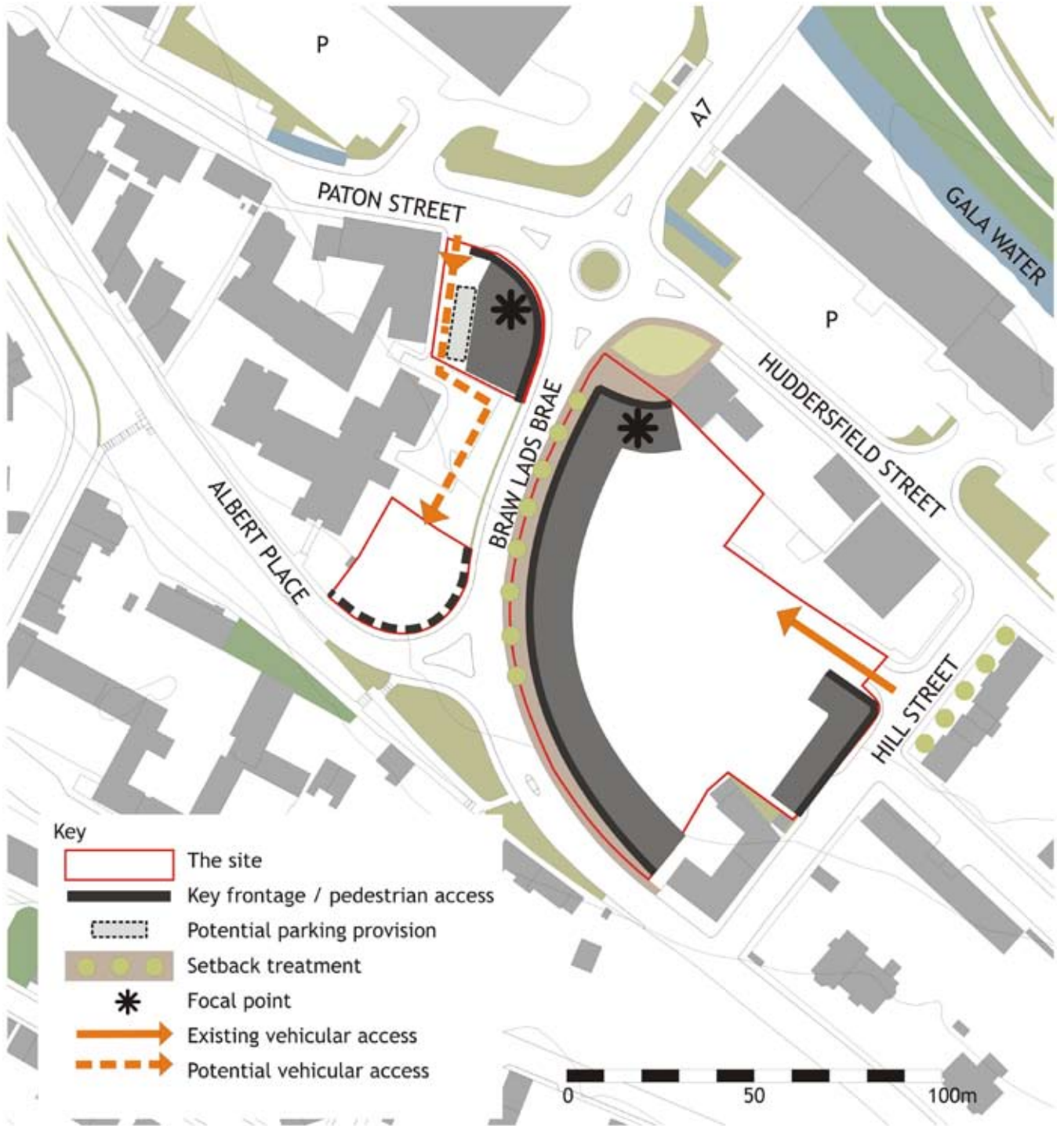


Figure 5 - Development Vision: Spatial Principles

## **2 Setback treatment**

- The building setback from the road along Braw Lads Brae should allow for 3m pavement with incorporated street furniture/planting to create a high quality attractive frontage to the street.
- The design should incorporate where appropriate a similarly high standard of setback treatment to the Hill Street frontage, including hard landscape and/or tree planting depending on the nature of the proposed development.

## **3 Storey heights**

- Building should avoid single storey development and could be up to 4 storeys high, relating to the landform, e.g. step down the hillside from the south to the north.

## **4 Access & Parking**

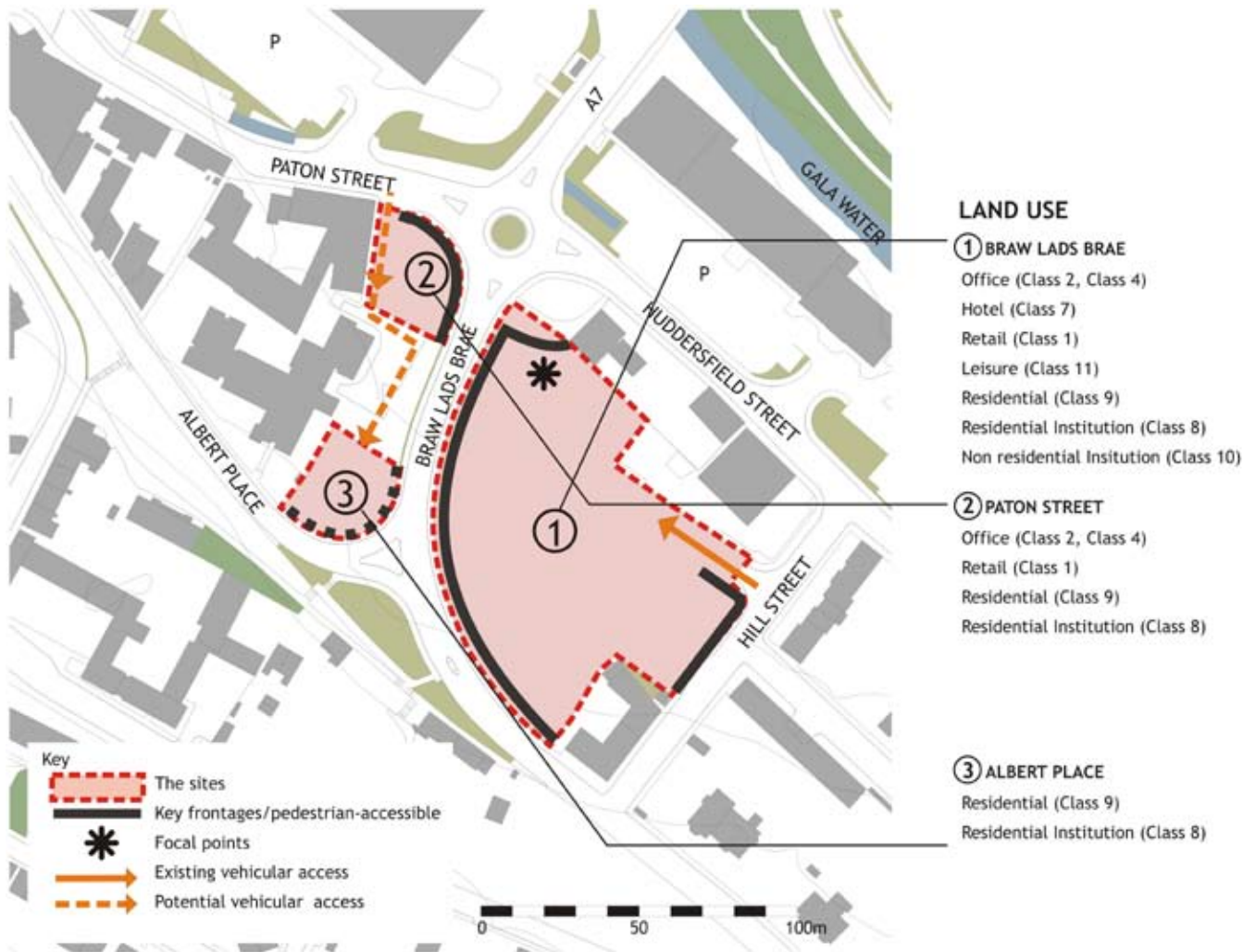
- Pedestrian access to the site at the corner of Huddersfield Street/Braw Lads Brae should be incorporated to connect with and utilise the public space created by the adjacent water feature.
- Vehicular access to the Braw Lads' Brae/Hill St site is taken from Hill Street.
- With regard to the vehicular servicing of the Paton St/Albert Place areas, west of Braw Lads Brae, this is constrained by the existing roads/levels arrangement. Potential access could be taken from Paton Street via the private lane serving the existing Abbotsford Court Housing, subject to agreement with Cairn Housing.
- Potential access could be explored from Albert Place to the adjacent site for service access and/or minimal parking.
- Pedestrian access to the building frontage along Braw Lads Brae should be incorporated as part of 'animating' the street.
- Internal streets within the Braw Lads Brae/Hill St site are to be designed in accordance with guidance set out in 'Designing Streets', the Scottish Government's policy statement.
- Parking levels should preferably be as detailed in Scottish Borders Council's Standard for Development Roads, the SEStran Parking Standards and the Local Plan and is dependant on the type of development proposed. Given the constrained nature of the site and the central location in the context of sustainable transport opportunities, consideration may be given to relaxation of parking standards provided that this can be demonstrated to be in wider interests of the proper planning of the area.
- Vehicular access must be accommodated to serve the rear of the adjacent properties on Hill Street.
- Please note that a Transport Assessment will be required for the redevelopment area, which will assess and resolve any issues relating to the local road network and sustainable travel patterns.

## **5 Key focal points**

- The development of the site must contribute positively to the existing townscape of Galashiels, serving as a link between the old and the new aspects of central Galashiels. There are key focal points that should be expressed in order to enhance this;
  - The corner sites around the Paton Street roundabout
  - The roofscape as viewed from surrounding steep valley sides to the north and south of the town centre
  - The curvature of the building line when viewed from the south and from along Albert Place/Braw Lads Brae
  - The frontage to Hill Street

## **6 Frontage activity**

- The development of the site should include the creation of buildings with active frontage to the street at ground level. This will be as appropriate to the end use, and can range from windows overlooking the street to pedestrian access to houses/shops/offices/etc as appropriate.



**Figure 6 - Preferred land use options across the site**

### Land Use Options

3.3 The site in its current form is deemed appropriate for development of the following uses, either singly or as a mixed use development, provided proposals are in accordance with Policy H3 and the exceptions outlined in paragraph 2.4:

- Office (Class 2, Class 4)
- Hotel (Class 7)
- Retail (Class 1)
- Leisure (Class 11)
- Residential (Class 9)
- Residential institution (Class 8)
- Non residential institution (Class 10)

Figure 6 shows where each class of development would be appropriate. An appraisal of the strengths, weaknesses, opportunities and threats was carried out on these uses and the implications for them on the site and surroundings. This is provided in the appendix.

### **Non-residential uses**

- 3.4 For non-residential uses, (Office, Hotel, Retail, Leisure & Non-residential Institution), the major implications will need to be addressed through appropriate studies. In this and the above policy contexts, proposals incorporating substantive retail development should be accompanied by a retail impact appraisal. It is noted that of the smaller two sites, only the one adjacent to Paton Street is deemed appropriate for some non-residential uses due to the location and access constraints.
- 3.5 Generally, though not exclusively, development contributions will not be sought in respect of proposed new business or industrial developments. Notwithstanding, it is recommended that commercial development proposals are discussed with the Development Negotiator at an early juncture to identify any potential contribution requirements.

### **Residential uses**

- 3.6 While a mixed use development would be preferable and would be consistent with the terms of the Local Plan, where a case has been made, and provided that other requirements of the SPG are met, single residential use may be appropriate. Any proposals will require to satisfy prevailing development contribution policy objectives. The nature and extent of such contribution requirements will reflect the anticipated impacts of development proposals upon service, infrastructure and facility provision. SBC publishes Supplementary Planning Guidance (SPG) to assist with the provisional assessment of contribution liabilities. SBC's Development Negotiator can also supply provisional assessments of policy implications if provided with relevant information.
- 3.7 The early clarification of development contribution policy implications is recommended in all instances.



## 4 SUSTAINABLE DESIGN

### Energy Efficient Design

- 4.1 The Scottish Borders Council Supplementary Planning Guidance (SPG) on Placemaking & Design sets out guidance on ‘designing out’ energy needs and creating sustainable new development. The SPG requires that new development is as efficient as is practicably possible in the use of natural and man-made resources. These principles apply to the building design, construction and operational energy requirements. This includes considering the site layout (microclimate, building orientation, water handling and use of sustainable resources) and the individual building design. Any new development on the site will have to demonstrate comprehensive integration of these principles to both the site layout and the individual building design.

### Renewable Energy

- 4.2 There is a Scottish Government commitment to increasing the amount of renewable energy generated through appropriately designed buildings (see Scottish Planning Policy and advice). Micro-renewable technologies can now be applied within the design of new housing development with confidence and should be incorporated where appropriate into development proposals.

- 4.3 The Scottish Borders Structure Plan (adopted by Scottish Ministers in 2001) Policy 121 “Small Scale Renewable Energy Technologies” states:

“Proposals for community and small scale renewable energy generation (or related techniques) will be encouraged where they have no significant adverse impact on the natural and built environment or amenity of the area.”

“All developers, whatever the nature of their proposals, will be encouraged to consider the potential to use materials, designs and technologies which either reduce the impact of energy consumption or reduce the environmental impact of energy generation when formulating proposals.”

- 4.4 The Local Plan (LP) 2011 Principle 1 (Sustainability) states:

“In determining planning applications and preparing development briefs, the Council will have regard to the following sustainability principles which underpin all the Plan’s policies and which developers will be expected to incorporate into their developments...”

“...(5) the efficient use of energy and resources, particularly non-renewable sources.”

- 4.5 The LP policy G1 (5) states:

“...in terms of the layout, orientation, construction and energy supply, the development (should) demonstrate that appropriate measures have been taken to maximise the efficient use of energy and resources, including the use of renewable energy and resources and the incorporation of sustainable construction techniques.”

### Energy statement

- 4.6 Scottish Borders Council requires an energy statement to be provided for development whose total floorspace is equal to or greater than 500sq. m. This should set out clearly how the building has been designed and will be constructed to minimise energy needs whilst maximising efficiency, as well as detailing any provision made for renewable energy. See Supplementary Planning Guidance (SPG): Renewable Energy (2007) for more details.

- 4.7 Developers must demonstrate how they comply with these policies.

## **5 ENVIRONMENTAL ASPECTS**

### **Water Resources and Waste Management**

- 5.1 There may be capacity constraints regarding the water supply network. Current waste water capacity may be adequate. A Drainage Impact Assessment will be required for this site. Consultation will be required with Scottish Water regarding the capacity of foul sewage disposal. Depending on the programme for development, development contributions may be required to ensure appropriate public foul sewer connection is achieved.

Much of the site is located in an elevated position above the existing flood plain. Consequently no flood risk assessment will be required (see the Galashiels Flood Study, March 2007).

An appropriate Sustainable Urban Drainage System (SUDS) is required to conform with SEPA, Scottish Water and the local planning authority standards and will require appropriate consultation. Consultation with SNH will also be necessary.

### **Archaeology**

- 5.2 The Burgh Yard incorporates various 19th century industrial buildings including a former millwrights' workshop, woollen and saw mills. A programme of archaeological evaluation and historic building recording was carried out in March 2007 and covered all land under SBC ownership. The results of these investigations will be made available to the site developer. Further archaeological monitoring will be required for ground breaking works on land currently outwith SBC ownership. Existing building stone should be retained for re-use in site walls.

### **Ecology**

- 5.3 To meet European and national legislation, and the requirements of the Council's Biodiversity policy, new development requires a bat and breeding bird survey. The Council's Ecology Officer should be contacted for more information (see contacts below). Bat and breeding bird surveys of existing buildings, which are to be removed, have been carried out in 2006-07. Remaining existing buildings will likewise need to be surveyed in advance of any development works. The appropriate design of bat boxes, bat bricks and bird boxes, including for swifts, needs to be incorporated into the design of the new buildings.

### **Pollution Remediation / Demolition**

- 5.4 The site contains contamination from historic industrial land uses such as the former Mill and, more recently, the former petrol station. The nature and extents of this was assessed as part of the GIRR 3 development, however further survey will be required. The specific manner in which SEPA requires its Pollution Prevention Guidelines to be implemented on this site will need to be observed.

## **Sustainability**

- 5.5 Any proposed development should incorporate provision for waste and recycling facilities in the layout and design of the site in accordance with PAN 63 Waste Management Planning.

The minimisation of waste during the construction phase should also be outlined as part of development proposals, demonstrating that:

- Construction practices minimise the use of raw materials and maximise the use of secondary aggregates and recycled or renewable materials;
- Waste material generated by the proposal is reduced and re-used or recycled where appropriate on site (for example in landscaping not resulting in excessive earth moulding and mounding). There may be opportunities to utilise surplus soils for sustainable purposes elsewhere.

## **6 DEVELOPMENT CONTRIBUTIONS**

- 6.1 Depending upon the nature and extent of development proposals, contributions towards the expansion of existing or new infrastructure, services and facilities may be required. SBC publishes Supplementary Planning Guidance (SPG) enabling stakeholders to provisionally assess contribution requirements. There are specific SPGs for:
- Development Contributions
  - Affordable Housing
  - Green Space

- 6.2 The Development Negotiator will also provide additional support to help identify and quantify development contribution policy implications.

At the time of writing, it is envisaged that contributions may be sought towards:

- Affordable Housing - 25% on-site provision
- Borders Railway Line Re-instatement
- St Peter's Primary School - new school rate
- Galashiels Academy - extension rate
- Central Borders Road Traffic Infrastructure
- Open Space - potential on-site provision
- Play Facility - potential on-site provision





## APPENDIX 1 SWOT ANALYSIS OF LAND USE OPTIONS

Hotel (Class 7)	
<b>STRENGTHS</b>	<b>WEAKNESSES</b>
<ul style="list-style-type: none"> <li>- Strategic location for regional/national access: A7/Borders Railway Line</li> <li>- Appropriate scale of site to accommodate parking</li> <li>- Potential to revitalise town centre with pedestrian traffic for retail/food and drink premises in the town</li> </ul>	<ul style="list-style-type: none"> <li>- Will generate additional vehicular traffic flows</li> </ul>
<b>OPPORTUNITIES</b>	<b>THREATS</b>
<ul style="list-style-type: none"> <li>- Strong visual presence within the town</li> <li>- Potential for additional compatible uses within site</li> <li>- Potential to attract 'budget' type hotel accommodation to the town.</li> </ul>	<ul style="list-style-type: none"> <li>- Ground contamination: appropriate remediation measures required</li> </ul>

Retail (Class 1)	
<b>STRENGTHS</b>	<b>WEAKNESSES</b>
<ul style="list-style-type: none"> <li>- Strategic location for access to A7/ Borders Railway Line</li> <li>- Potential to enhance retail 'offer' of town centre</li> <li>- Synergy with adjacent retail activity around existing shopping centre</li> </ul>	<ul style="list-style-type: none"> <li>- May cause migration of business from town centre</li> <li>- Will generate additional vehicular traffic</li> <li>- Limited scope to accommodate large-floorplate retail units within the required urban form</li> </ul>
<b>OPPORTUNITIES</b>	<b>THREATS</b>
<ul style="list-style-type: none"> <li>- Potential for ancillary retail in conjunction with other compatible uses</li> <li>- Potential for some larger scale retail, provided it can be delivered in line with spatial principles outlined above</li> </ul>	<ul style="list-style-type: none"> <li>- Could adversely impact on Paton St roundabout and Paton St road network</li> <li>- Retail saturation a possible threat to current town centre vitality</li> <li>- Ground contamination: appropriate remediation measures required</li> </ul>

Leisure, Food & Drink (Class 11, Class 3)	
<b>STRENGTHS</b>	<b>WEAKNESSES</b>
<ul style="list-style-type: none"> <li>- Strategic location for regional access: A7/ Borders railway Line</li> <li>- Potential to invigorate town centre 'offer' with additional activities available</li> <li>- Synergy with adjacent retail activity around existing shopping centre</li> </ul>	<ul style="list-style-type: none"> <li>- May exacerbate perceived threat to original town centre retail activity</li> <li>- Will generate additional vehicular traffic</li> </ul>
<b>OPPORTUNITIES</b>	<b>THREATS</b>
<ul style="list-style-type: none"> <li>- Potential for additional compatible uses such as ancillary retail</li> </ul>	<ul style="list-style-type: none"> <li>- Ground contamination: appropriate remediation measures required</li> </ul>

Residential (Class 9)	
<b>STRENGTHS</b>	<b>WEAKNESSES</b>
<ul style="list-style-type: none"> <li>- Strategic location for access to local services/sustainable transport</li> <li>- Brings people into town centre: enhanced civic vitality</li> </ul>	<ul style="list-style-type: none"> <li>- Will generate additional vehicular traffic, however this will be less than more peripheral housing sites</li> </ul>
<b>OPPORTUNITIES</b>	<b>THREATS</b>
<ul style="list-style-type: none"> <li>- Potential for additional compatible uses such as ancillary retail</li> <li>- Potential to revitalise town centre with enhanced pedestrian footfall</li> </ul>	<ul style="list-style-type: none"> <li>- Ground contamination: appropriate remediation measures required</li> </ul>

Residential institution (Class 8)	
<b>STRENGTHS</b>	<b>WEAKNESSES</b>
<ul style="list-style-type: none"> <li>- Strategic location for access to local services/sustainable transport</li> <li>- Brings people into town centre: enhanced civic vitality</li> <li>- Potential for lower level of associated car use</li> </ul>	<ul style="list-style-type: none"> <li>- Will generate some additional vehicular traffic, however this will be less than private housing and less than that generated by more peripheral housing sites</li> </ul>
<b>OPPORTUNITIES</b>	<b>THREATS</b>
<ul style="list-style-type: none"> <li>- Halls of residence/sheltered housing within central Galashiels</li> </ul>	<ul style="list-style-type: none"> <li>- Ground contamination: appropriate remediation measures required</li> </ul>

Non residential institution (Class 10)	
<b>STRENGTHS</b>	<b>WEAKNESSES</b>
<ul style="list-style-type: none"> <li>- Strategic location for access to local community/wider sustainable transport network</li> <li>- Potential to invigorate town centre vitality with additional leisure/community uses</li> </ul>	<ul style="list-style-type: none"> <li>- Will generate some additional vehicular traffic</li> </ul>
<b>OPPORTUNITIES</b>	<b>THREATS</b>
<ul style="list-style-type: none"> <li>- Potential for additional compatible uses such as ancillary retail</li> </ul>	<ul style="list-style-type: none"> <li>- Ground contamination: appropriate remediation measures required</li> </ul>

## APPENDIX 2 POLICY CONTEXT

### Scottish Government

#### Designing Places

- Designing Streets
- Scottish Planning Policy (SPP) 1: The Planning System
- SPP 3: Planning for Homes (revised 2008)
- SPP 6: Renewable Energy
- SPP 17: Planning for Transport
- SPP 23 Planning and the Historic Environment
- Planning Advice Note (PAN) 33: Development of Contaminated Land
- PAN 42: Archaeology - the Planning Process and Scheduled Monument Procedures
- PAN 45 Annex 1: Planning for Micro Renewables
- PAN 51: Planning, Environmental Protection and Regulation
- PAN 52: Planning in Small Towns
- PAN 61: Planning & Sustainable Urban Drainage Systems
- PAN 65: Planning and Open Space
- PAN 67: Housing Quality
- PAN 68: Design Statements
- PAN 77: Designing Safer Places
- PAN 78: Inclusive Design
- PAN 2/2010: Affordable Housing & Housing Land Audits

### Scottish Borders Structure Plan 2001–2018

- Policy N16: Archaeological Evaluation, Preservation & Recording
- Policy N20: Design
- Policy H7: Affordable & Special Needs Housing - Proportion
- Policy H8: Affordable & Special Needs Housing - Assessment
- Policy C6: Open Space
- Policy C7: Play Areas
- Policy C8: Access Network
- Policy I5: Cycling
- Policy I7: Walking
- Policy I11: Parking Provision in New Development
- Policy I15: Flood Risk Areas

### Scottish Borders Consolidated Local Plan (2011)

- Policy G1: Quality Standards for New Development
- Policy G4: Flooding
- Policy G5: Developer Contributions
- Policy BE2: Archaeological Sites & Ancient Monuments
- Policy NE3: Local Biodiversity
- Policy NE4: Trees, Woodlands & Hedgerows
- Policy NE5: Development Affecting the Water Environment
- Policy H1: Affordable Housing
- Policy Inf2: Protection of Access Routes
- Policy Inf3: Road Adoption Standards
- Policy Inf4: Parking Standards
- Policy Inf6: Sustainable Urban Drainage
- Supplementary Planning Guidance (SPG) Placemaking & Design
- SPG Affordable Housing
- SPG Developer Contributions
- SPG Biodiversity
- SPG Designing Out Crime in the Scottish Borders
- SPG Renewable Energy

SEPA Pollution Prevention Guidelines  
PPG 1 General guide to the prevention of pollution

Trees/Landscape BS Standard 5837: 2005 'Trees in Relation to Construction' (Arboricultural Association 2005).

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